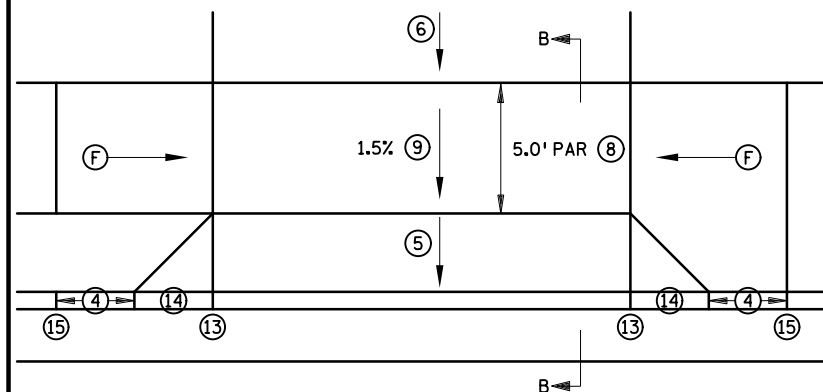
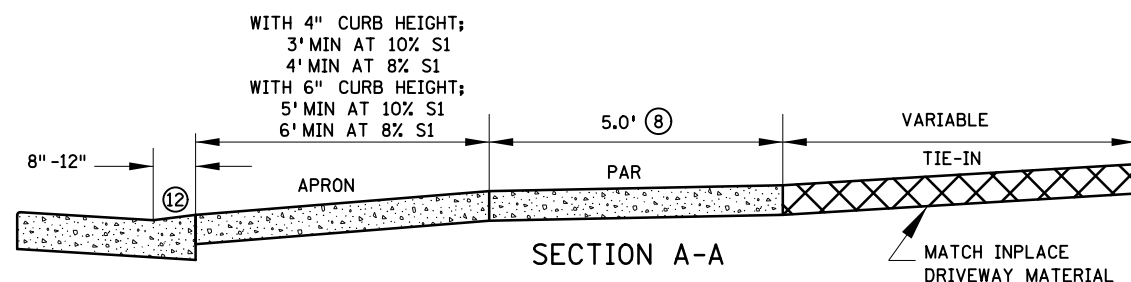


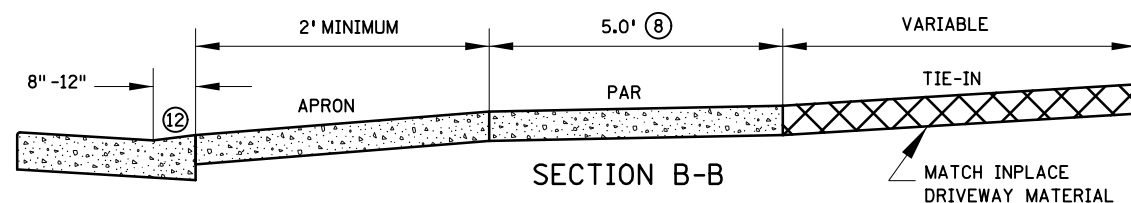
PERPENDICULAR DRIVEWAY ①



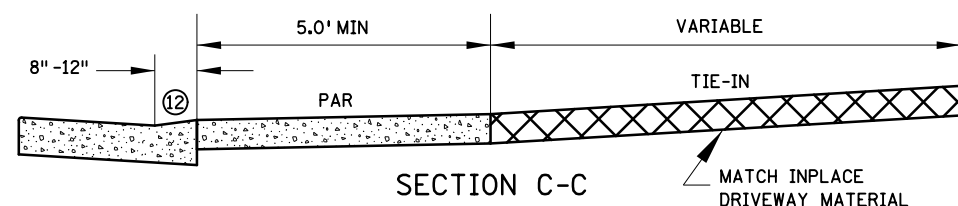
TIERED PERPENDICULAR DRIVEWAY ②



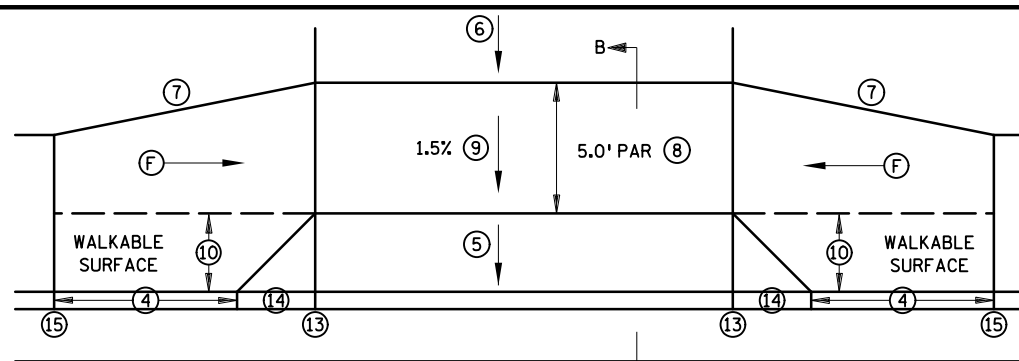
SECTION A-A



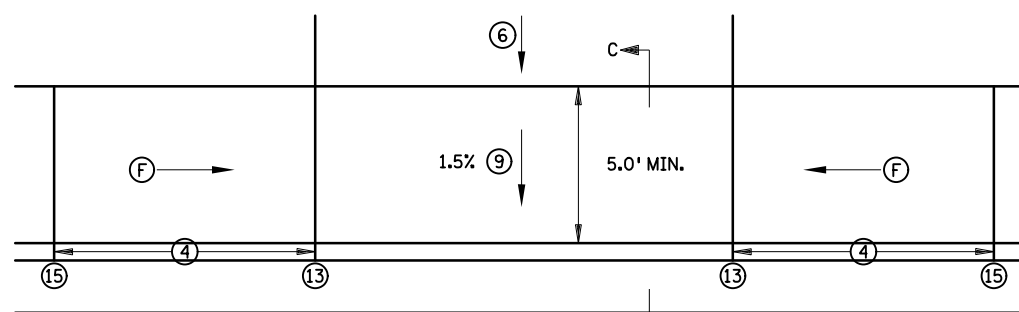
SECTION B-B



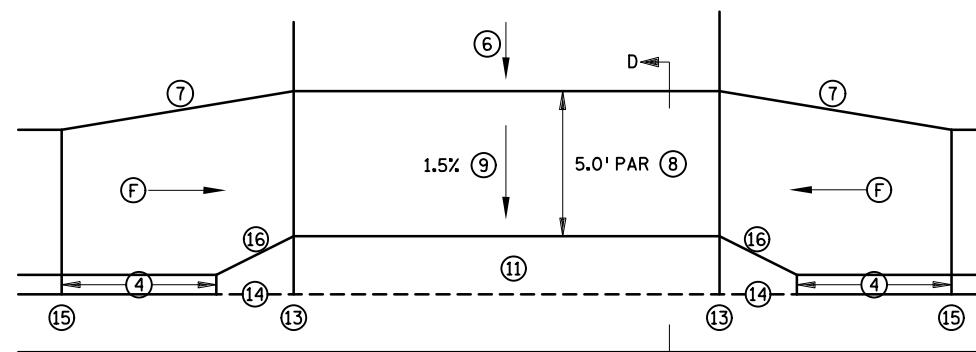
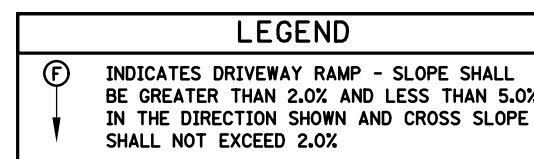
SECTION C-C



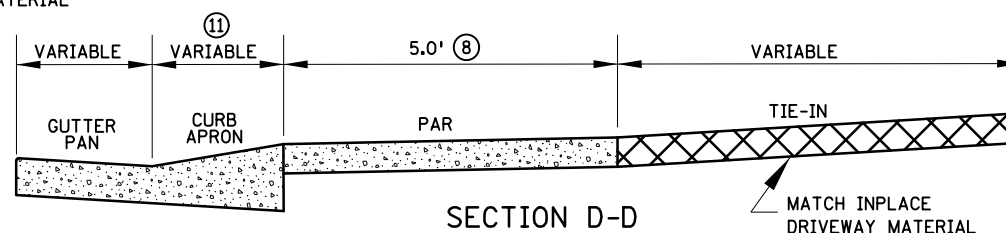
TIERED PERPENDICULAR OFFSET DRIVEWAY ②



PARALLEL DRIVEWAY ③



INTEGRAL DRIVEWAY APRON



SECTION D-D

NOTES:

ALL SIDEWALK AND BOULEVARD WIDTHS SHALL BE MEASURED FROM BACK OF CURB.

IN URBAN ROADWAY SECTIONS, 6" CURB HEIGHT SHOULD BE USED WHEN 6' OR GREATER BOULEVARD WIDTH IS PROPOSED. WHEN BOULEVARD IS LESS THAN 6' WIDE, 4" CURB HEIGHT SHOULD BE USED.

MAINTAIN EXISTING DRAINAGE PATTERNS FLOWING TO PUBLIC RIGHT OF WAY.

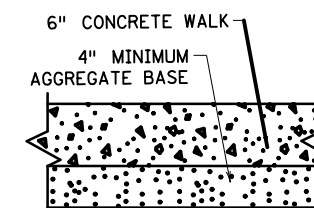
ACQUIRE ADEQUATE L3 TO ALLOW FOR A CONTINUOUS PAR PROFILE (UNIFORM TYPICAL SIDEWALK SECTION) THROUGH THE DRIVEWAY APRON.

IN NO CASE SHALL SIDEWALK PROFILES EXCEED 5.0%, EXCEPT SIDEWALK PROFILES CAN MATCH ROADWAY GRADE IF ROADWAY GRADE IS GREATER THAN 5.0%. RAMP FOR DRIVEWAYS ARE REQUIRED TO FOLLOW THE ABOVE SIDEWALK CRITERIA.

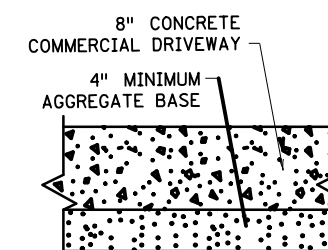
CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PEDESTRIAN ACCESS ROUTE (PAR). 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOPS OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.

DRIVEWAY TYPES FROM MOST PREFERRED TO LEAST PREFERRED ARE AS FOLLOWS: PERPENDICULAR, TIERED PERPENDICULAR, TIERED PERPENDICULAR OFFSET & PARALLEL.

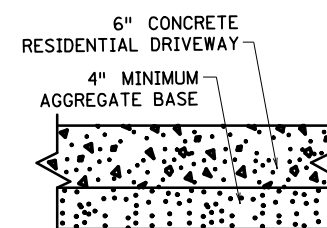
- ① PERPENDICULAR DRIVEWAYS ARE THE STANDARD AND STARTING POINT FOR ALL DRIVEWAY DESIGN AND CONSTRUCTION. SHOULD BE USED TO ACHIEVE CONTINUOUS PAR PROFILE THROUGH THE DRIVEWAY. OBTAINING A PERPENDICULAR DRIVEWAY DESIGN BECOMES MORE CRITICAL WITH STEEP ROADWAY PROFILES.
- ② TO BE USED WHEN PERPENDICULAR DRIVEWAY DESIGN CANNOT BE ACHIEVED, THE DRIVEWAY PAR IS BELOW ROADWAY CURB HEIGHT. THIS DRIVEWAY TYPE CAN BE USED FOR BOTH PAVED (AS SHOWN) AND GRASS BOULEVARDS.
- ③ TO BE USED WHEN PERPENDICULAR AND TIERED PERPENDICULAR DRIVEWAY DESIGN CANNOT BE ACHIEVED. CAN BE USED FOR STEEP NEGATIVE SLOPED DRIVEWAYS. DW CURB TYPE 2 SHOULD BE USED TO RAISE PAR ABOVE GUTTER AND REDUCE "ROLLER COASTER" EFFECT. 4" HIGH ROADWAY CURB SHOULD BE USED TO REDUCE "ROLLER COASTER" EFFECT ESPECIALLY WHEN MULTIPLE DRIVEWAYS ARE PRESENT.
- ④ TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.
- ⑤ 8% STANDARD, 10% MAX. FOR COMMERCIAL AND 12% MAX. FOR RESIDENTIAL. SEE GENERAL NOTES ON SHEET 2 FOR MORE INFORMATION.
- ⑥ S3 8% MAXIMUM, IF THE SLOPE IS EXCEEDED OR CONTINUED FOR MORE THAN 5', ANALYZE VEHICLE TEMPLATES FOR VERTICAL CLEARANCE. IF EXISTING DRIVEWAY IS NEGATIVELY DRAINING, S3 CAN BECOME SLIGHTLY MORE NEGATIVE TO ACHIEVE PERPENDICULAR DRIVEWAY DESIGN IF THE VERTICAL CLEARANCE IS ACHIEVED IN VEHICLE TEMPLATES.
- ⑦ 1:3 MIN. 1:5 PREFERRED FOR DRIVEWAY RETROFIT PROJECTS. 1:10 PREFERRED FOR SIDEWALK REPLACEMENT PROJECTS.
- ⑧ 5.0' MIN. PAR WIDTH IS THE STANDARD THROUGH DRIVEWAYS. IF FEASIBLE WIDEN DRIVEWAY PAR WIDTH TO MATCH APPROACHING SIDEWALK PAR WIDTHS. IN VERTICALLY CONSTRAINED AREAS PAR WIDTHS CAN INCREMENTALLY BE REDUCED TO 4.5' OR 4' MIN AFTER ALL OTHER OPTIONS HAVE BEEN APPLIED.
- ⑨ THE PEDESTRIAN ACCESS ROUTE, MAY NOT EXCEED 0.02 FT./FT. AS CONSTRUCTED.
- ⑩ SIDEWALK OFFSET TO BE LESS THAN OR EQUAL TO HALF THE APPROACHING SIDEWALK WIDTH.
- ⑪ INTEGRAL DRIVEWAY APRON TO BE POURED MONOLITHICALLY/INTEGRAL WITH THE CURB AND GUTTER. SEE SHEET 2 FOR MORE INFORMATION.
- ⑫ SEE SHEET 2 FOR CURB TYPE INFORMATION.
- ⑬ 0" CURB IS AT FLOW LINE. SEE DRIVEWAY TABLE FOR BACK OF CURB HEIGHTS.
- ⑭ 3' LONG AT 8-10% PREFERRED FOR INITIAL CURB TAPER. REDUCE CURB TAPER SLOPE IF NECESSARY TO MATCH ADJACENT SIDEWALK GRADES.
- ⑮ MATCH FULL CURB HEIGHT.
- ⑯ 1:2 TAPER RATE ON INTEGRAL DRIVEWAY APRONS.
- ⑰ SEE SHEET 4 FOR WHEN 6" WALK IS REQUIRED.



TYPICAL SIDEWALK SECTION ⑰



8" CONCRETE
COMMERCIAL DRIVEWAY
4" MINIMUM
AGGREGATE BASE



6" CONCRETE
RESIDENTIAL DRIVEWAY
4" MINIMUM
AGGREGATE BASE

TYPICAL DRIVEWAY SECTIONS

REVISION:

APPROVED: 11-04-2021

Jeffrey J. Perkins
OPERATIONS DIVISION



STANDARD PLAN 5-297.254

1 OF 4

THOMAS STYRBICKI
STATE DESIGN ENGINEER

APPROVED: 11-04-2021
REVISED:

STATE PROJ. NO.

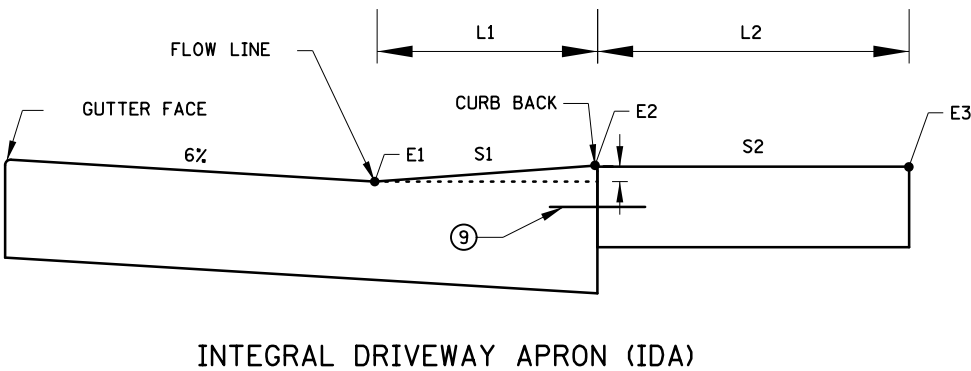
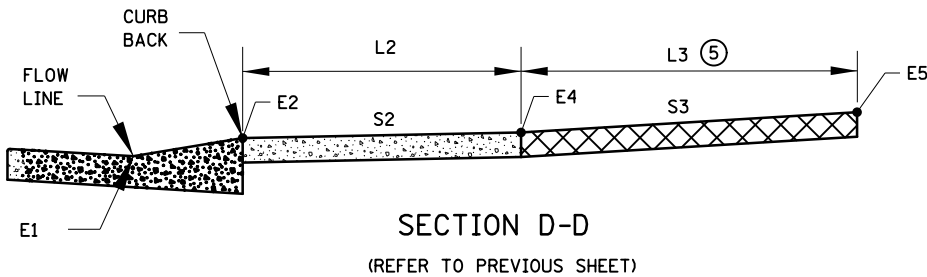
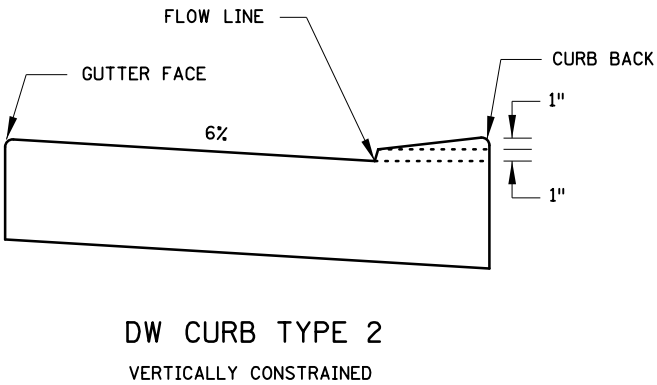
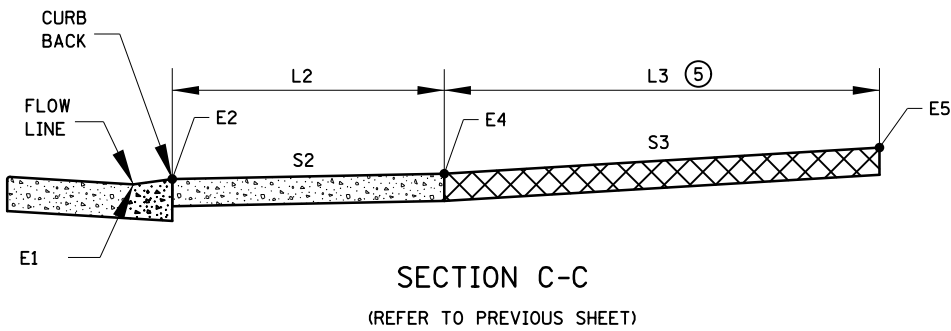
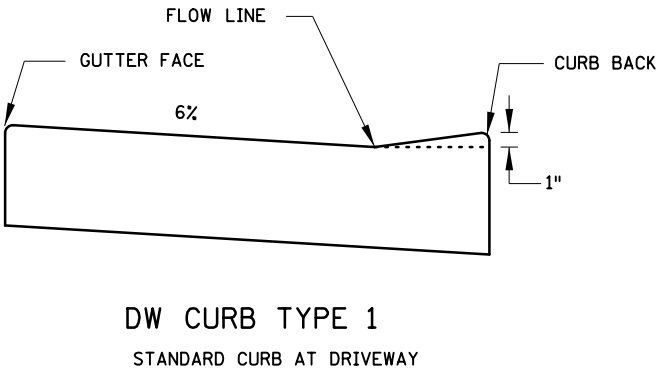
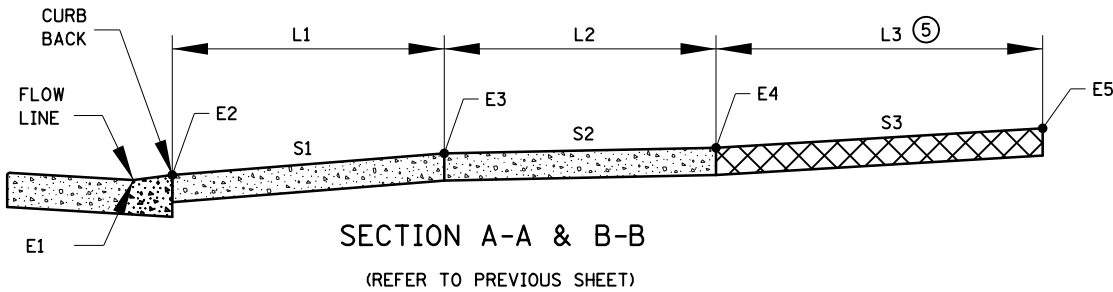
DRIVEWAY AND SIDEWALK DETAILS

(TH) SHEET NO. OF SHEETS

DRIVEWAY TABLE ①																
STATION	SIDE	DRIVEWAY TYPE ②	CURB TYPE ③	E1	E2	L1	S1	E3	L2	S2 ④	E4	L3 ⑤	S3	EXISTING ⑥	E5	COMMENTS
						FT	%		FT	%		FT	%	%		

NOTES:

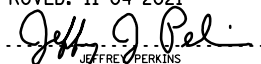
- ALL SIDEWALK AND BOULEVARD WIDTHS SHALL BE MEASURED FROM BACK OF CURB.
- DW CURB TYPE 1 SHALL BE USED WHEN THE DRIVEWAY ACTS AS A PEDESTRIAN RAMP. THE MAX. APRON SLOPE MUST ADHERE TO ADA CRITERIA AS WELL. DW CURB TYPE 1 SHOULD BE USED IF THERE IS ON STREET PARKING.
- WHERE ROADWAY DRAINAGE IS A CONCERN (NEGATIVE SLOPED APRON) DW CURB TYPE 2 CAN BE USED TO HELP KEEP THE WATER ON PUBLIC RIGHT OF WAY.
- S1 8% STANDARD, 10% MAX. COMMERCIAL AND 12% MAX. RESIDENTIAL. IF EXISTING GRADES ARE STEEPER DO NOT MAKE GRADES APPRECIABLY WORSE BY USING BEST PRACTICES SUCH AS DRIVEWAY CURB HEIGHTS, EXTENDING L3 AND/OR STEEPEN S3.
- S3 8% MAXIMUM, IF THIS SLOPE IS EXCEEDED OR CONTINUED FOR MORE THAN 5', ANALYZE VEHICLE TEMPLATES FOR VERTICAL CLEARANCE. SEE FACILITY DESIGN GUIDE, CHAPTER 6, FOR GEOMETRIC DESIGNS OF DRIVEWAYS.
- ① EXAMPLE SHOWN TO BE INCLUDED IN PLAN FOR EACH DRIVEWAY THAT HAS PAR THROUGH IT.
- ② REFERS TO THE FOLLOWING TYPES; PERPENDICULAR DRIVEWAY, TIERED PERPENDICULAR OFFSET DRIVEWAY, TIERED PERPENDICULAR DRIVEWAY, PARALLEL DRIVEWAY, AND INTEGRAL DRIVEWAY APRON.
- ③ DW CURB TYPE 1 IS THE STANDARD AND SHALL BE THE STARTING POINT FOR ALL PERPENDICULAR AND TIERED DRIVEWAYS. DW CURB TYPE 2 SHALL ONLY BE USED AFTER UTILIZING BEST PRACTICES SUCH AS MAXIMIZING S1, S3, AND L3.
- ④ SHOULD BE DESIGNED AT 1.5%.
- ⑤ ACQUIRE ADEQUATE L3 TO ALLOW FOR CONTINUOUS PAR PROFILE (UNIFORM SIDEWALK SECTION) THROUGH THE DRIVEWAY APRON.
- ⑥ PROVIDE INPLACE TIE-IN SLOPE INFORMATION AT BACK OF PROPOSED WALK (S3 AREA).
- ⑦ INFORMATION TO BE INCORPORATED INTO DRIVEWAY TABLE WHEN INTEGRAL DRIVEWAY APRON IS USED. OTHER CURB HEIGHTS & CURB APRON LENGTHS CAN BE USED.
- ⑧ L1 & S1 FOR INTEGRAL DRIVEWAY APRON IS TO FLOWLINE. 12.5% IS MAXIMUM PREFERRED SLOPE.
- ⑨ TIE ADJACENT SECTIONS. CONCRETE DRIVEWAY APRON AND CONCRETE DRIVEWAY SIDEWALK SHALL BE CONSTRUCTED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. DRILL AND GROUT OR CAST IN-PLACE THROUGH HOLES IN THE FORMS NO. 4 X 12" LONG TIE BARS (EPOXY COATED). 36" MAXIMUM SPACING WITH 2" MINIMUM CONCRETE COVER PLACED 1' MINIMUM FROM ADJACENT CONSTRUCTION JOINT.



TYPICAL INTEGRAL DRIVEWAY APRON ⑦			
CURB TYPE	L1	E2	S1 ⑧
	FT		%
IDA 216	1.33	+0.16	12.5
IDA 220	1.67	+0.16	10
IDA 324	2	+0.24	12.5
IDA 432	2.67	+0.33	12.5

REVISION:

APPROVED: 11-04-2021


JEFFREY J. PERKINS
OPERATIONS DIVISION



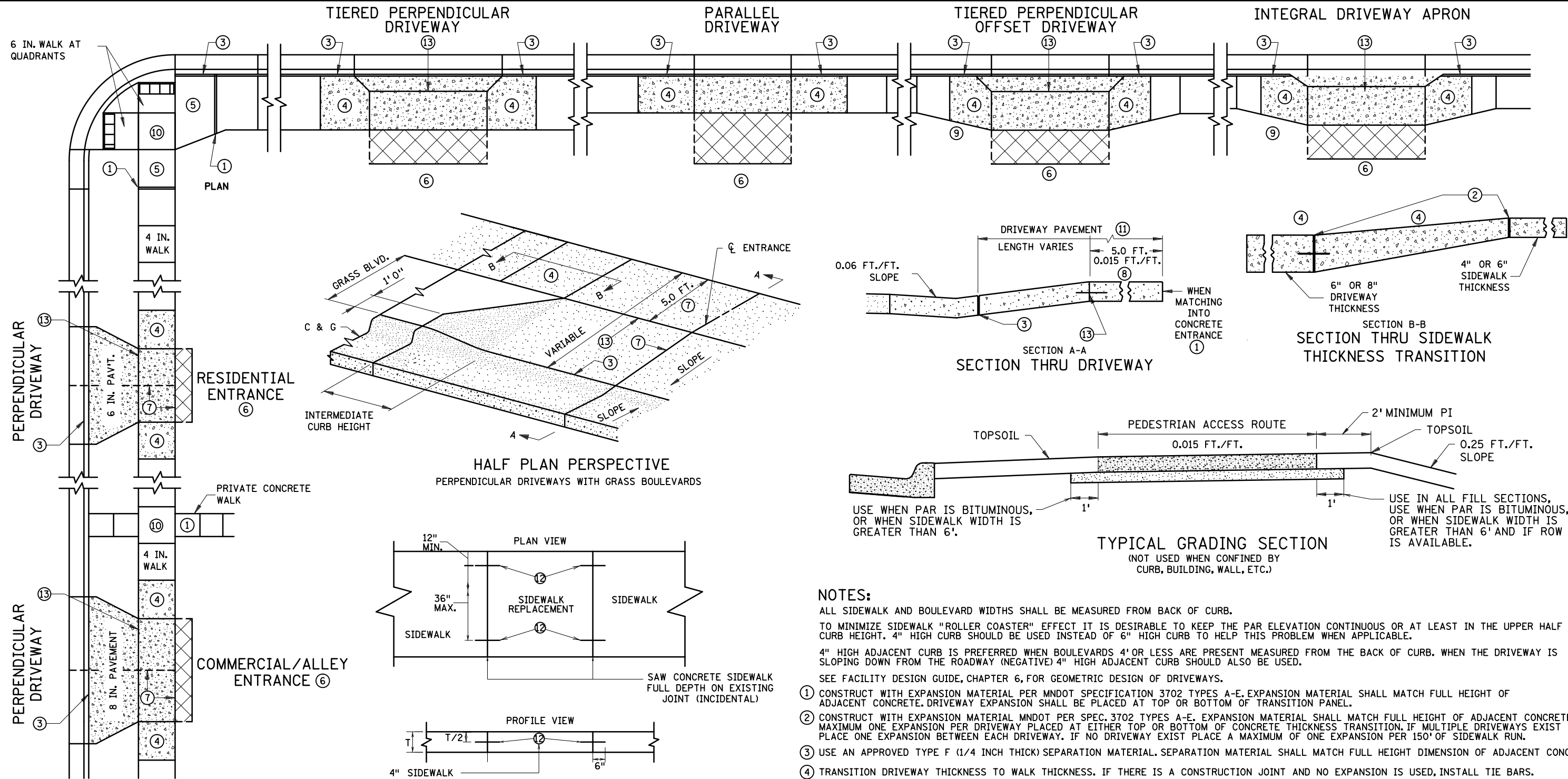
STANDARD PLAN 5-297.2542 OF 4


THOMAS J. TYRBICKI
STATE DESIGN ENGINEER

APPROVED: 11-04-2021
REVISED:

DRIVEWAY AND SIDEWALK DETAILS

STATE PROJ. NO. (TH) SHEET NO. OF SHEETS



SIDEWALK LONGITUDINAL JOINT TIE BAR TABLE				
SIDEWALK WIDTH, W	SIDEWALK THICKNESS, T	TIE BAR SIZE	LENGTH	SPACING
> 7'	4"	No. 4	12"	24"
>10'	6"	No. 4	12"	36"

FOR 4" CONCRETE ONLY: CAST IN PLACE BARS MUST BE SUPPORTED WITH P-STAKES OR REINFORCEMENT BASKETS FOR FULL WIDTH CONCRETE PLACEMENTS.

FOR 6" CONCRETE ONLY: DRILL AND GROUT OR CAST IN PLACE THROUGH HOLES IN THE FORMS REQUIRED FOR STAGED ADJACENT CONCRETE PLACEMENTS.

- NOTES:**
- ALL SIDEWALK AND BOULEVARD WIDTHS SHALL BE MEASURED FROM BACK OF CURB.
- TO MINIMIZE SIDEWALK "ROLLER COASTER" EFFECT IT IS DESIRABLE TO KEEP THE PAR ELEVATION CONTINUOUS OR AT LEAST IN THE UPPER HALF OF CURB HEIGHT. 4" HIGH CURB SHOULD BE USED INSTEAD OF 6" HIGH CURB TO HELP THIS PROBLEM WHEN APPLICABLE.
- 4" HIGH ADJACENT CURB IS PREFERRED WHEN BOULEVARDS 4' OR LESS ARE PRESENT MEASURED FROM THE BACK OF CURB. WHEN THE DRIVEWAY IS SLOPING DOWN FROM THE ROADWAY (NEGATIVE) 4" HIGH ADJACENT CURB SHOULD ALSO BE USED.
- SEE FACILITY DESIGN GUIDE, CHAPTER 6, FOR GEOMETRIC DESIGN OF DRIVEWAYS.
- CONSTRUCT WITH EXPANSION MATERIAL PER MNDOT SPECIFICATION 3702 TYPES A-E. EXPANSION MATERIAL SHALL MATCH FULL HEIGHT OF ADJACENT CONCRETE. DRIVEWAY EXPANSION SHALL BE PLACED AT TOP OR BOTTOM OF TRANSITION PANEL.
 - CONSTRUCT WITH EXPANSION MATERIAL MNDOT PER SPEC. 3702 TYPES A-E. EXPANSION MATERIAL SHALL MATCH FULL HEIGHT OF ADJACENT CONCRETE. MAXIMUM ONE EXPANSION PER DRIVEWAY PLACED AT EITHER TOP OR BOTTOM OF CONCRETE THICKNESS TRANSITION. IF MULTIPLE DRIVEWAYS EXIST PLACE ONE EXPANSION BETWEEN EACH DRIVEWAY. IF NO DRIVEWAY EXIST PLACE A MAXIMUM OF ONE EXPANSION PER 150' OF SIDEWALK RUN.
 - USE AN APPROVED TYPE F (1/4 INCH THICK) SEPARATION MATERIAL. SEPARATION MATERIAL SHALL MATCH FULL HEIGHT DIMENSION OF ADJACENT CONCRETE.
 - TRANSITION DRIVEWAY THICKNESS TO WALK THICKNESS. IF THERE IS A CONSTRUCTION JOINT AND NO EXPANSION IS USED, INSTALL TIE BARS.
 - TRANSITION CURB RAMP THICKNESS TO WALK THICKNESS.
 - MATCH INPLACE DRIVEWAY WIDTH, MATERIAL TYPE AND THICKNESS.
 - FORM CONTRACTION JOINT AS NEEDED TO PRODUCE APPROXIMATELY SQUARE PANELS. CONCRETE PANEL SIZE SHOULD NOT EXCEED 1 1/2 : 1 LENGTH X WIDTH. 81 SF FOR 6" CONCRETE DRIVEWAY WITH 9'X9' MAXIMUM PANEL SIZE. 144 SF FOR 8" CONCRETE DRIVEWAY WITH 12'X12' MAXIMUM PANEL SIZE. MATCH DRIVEWAY APRON AND SIDEWALK JOINTS.
 - THE PEDESTRIAN ACCESS ROUTE CROSS-SLOPE, SHALL NOT EXCEED 0.02 FT./FT. AS CONSTRUCTED.
 - 1:10 MIN. SIDEWALK OFFSET TAPER REQUIRED FOR SIDEWALK REPLACEMENT PROJECTS. 1:3 MIN. AND 1:5 MIN. PREFERRED SIDEWALK OFFSET TAPER FOR DRIVEWAY REPLACEMENT.
 - LANDING REQUIRED, SEE NEXT SHEET FOR MORE INFORMATION.
 - CONCRETE DRIVEWAY APRON AND CONCRETE DRIVEWAY SIDEWALK SECTIONS SHALL BE CONSTRUCTED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. ENGINEER'S APPROVAL REQUIRED FOR MONOLITHIC PLACEMENTS.
 - DRILL AND GROUT NO. 4 X 12" LONG TIE BARS (EPOXY COATED). 36" MAXIMUM SPACING BETWEEN BARS COVER PLACED 1' MINIMUM FROM ADJACENT CONSTRUCTION JOINTS. 1' MINIMUM FROM ADJACENT CONCRETE JOINTS. BARS TO BE ADJUSTED TO MATCH SIDEWALK GRADES. TO BE PAID BY EACH.
 - DRILL AND GROUT OR CAST IN-PLACE THROUGH HOLES IN THE FORMS NO. 4 X 12" LONG TIE BARS (EPOXY COATED). 36" MAXIMUM SPACING BETWEEN BARS WITH 2" MINIMUM CONCRETE COVER PLACED 1' MINIMUM FROM ADJACENT CONSTRUCTION JOINTS. 1' MINIMUM FROM ADJACENT CONCRETE JOINTS.

REVISION: 12-23-2021

APPROVED: 11-04-2021

Jeffrey J. Perkins
JEFFREY J. PERKINS
OPERATIONS DIVISION

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MINNESOTA
DEPARTMENT
OF
TRANSPORTATION

STANDARD PLAN 5-297.254

3 OF 4

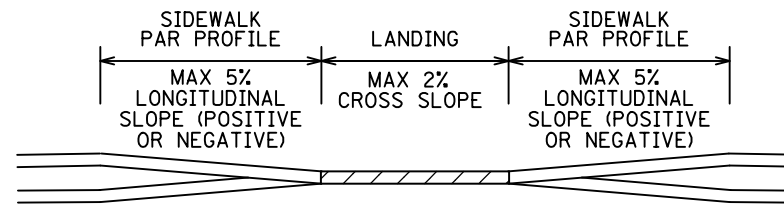
Tom Styrbicki
THOMAS STYRBICKI
STATE DESIGN ENGINEER

APPROVED: 11-04-2021
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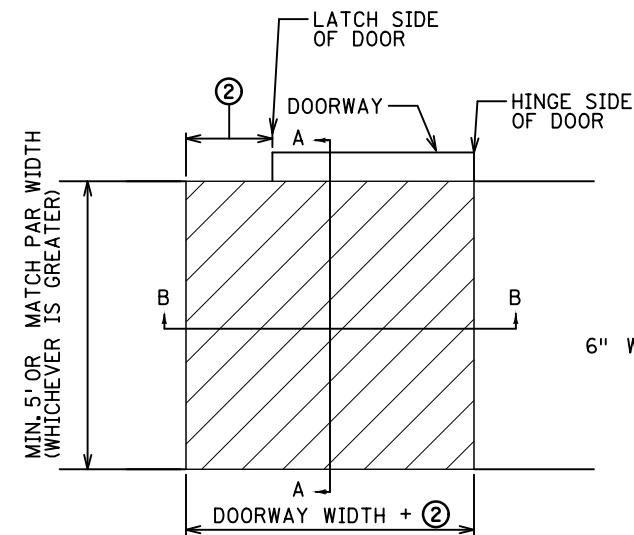
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DRIVEWAY AND SIDEWALK DETAILS

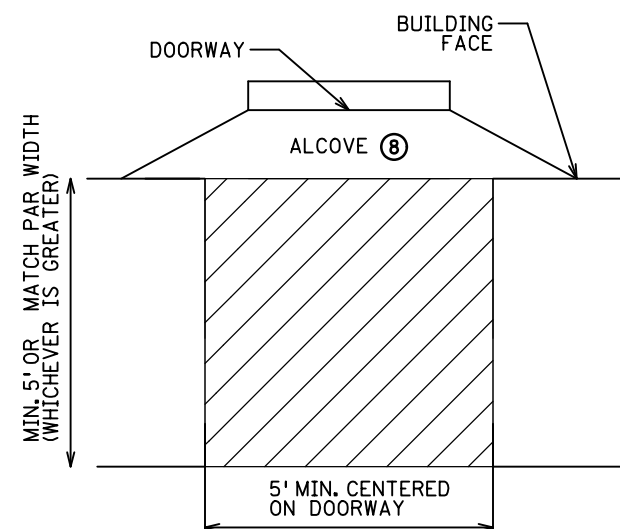
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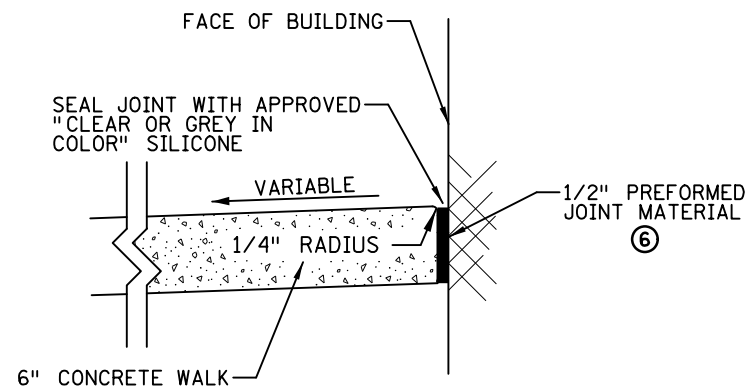
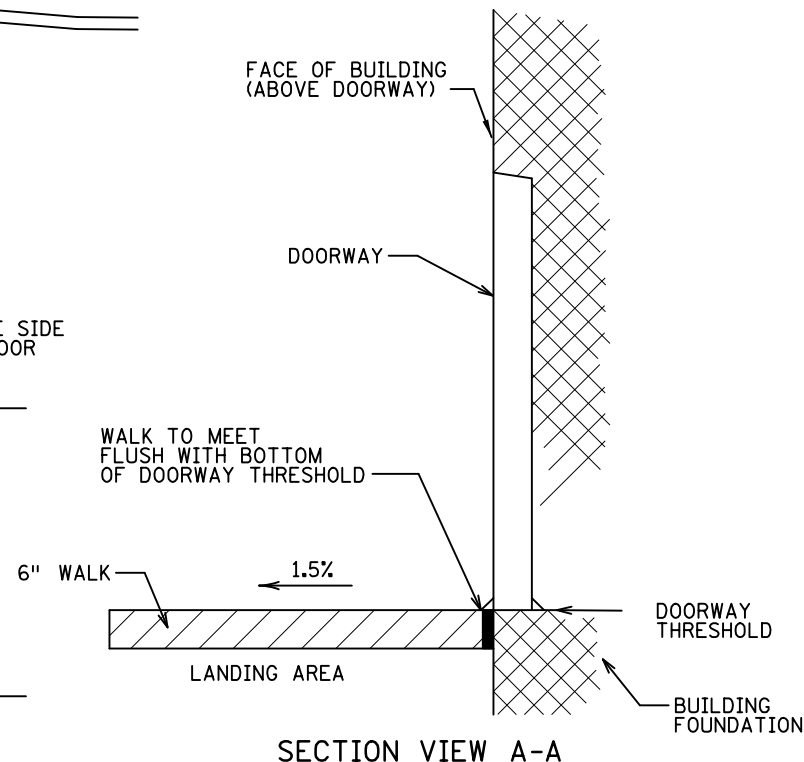
SECTION VIEW B-B



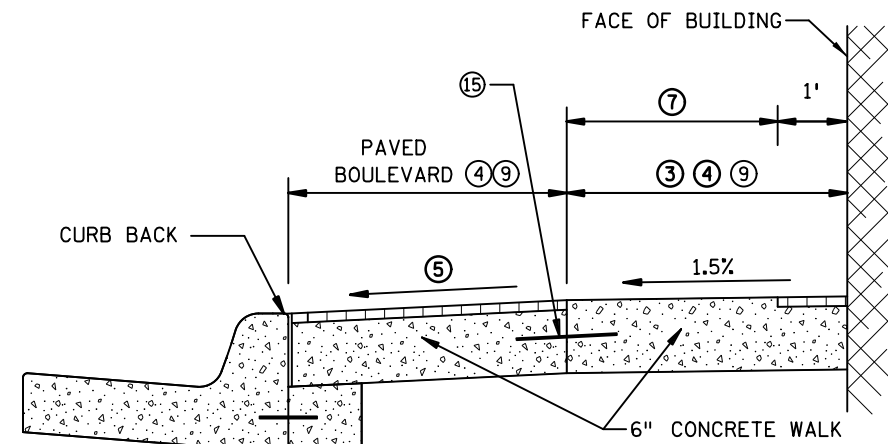
PLAN VIEW DOORWAY



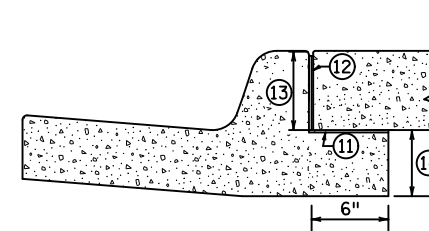
PLAN VIEW DOORWAY WITH ALCOVE
SIDEWALK LANDING REQUIREMENTS ①



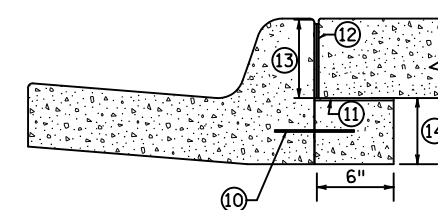
BUILDING JOINT SEAL (INCIDENTAL)



DOWNTOWN SIDEWALK TYPICAL SECTION



SLIP FORM SILL



FIXED FORM SILL

SILL CURB SHOULD BE USED AT ALL LOCATIONS WHEN CONCRETE WALK IS AT BACK OF CURB, INCLUDING PAVED BOULEVARD.
SILL CURB SHALL NOT BE USED IN CURB RAMP AND DRIVEWAY AREAS, INCLUDING CONCRETE FLARES.
SILL CURB WITH 4" WALK CAN USE FIXED OR SLIP FORM OPTIONS.

NOTES:

- 6" WALK IS REQUIRED:
1) IN ALL SIDEWALK LOCATIONS WHERE VARIABLE SLOPED CONCRETE BOULEVARDS ARE PAVED, SUCH AS COMMERCIAL (STORE FRONT, DOWNTOWN) AREAS.
2) ANYTIME DRILL AND REINFORCEMENT IS USED TO TIE LONGITUDINAL JOINTS TOGETHER.
3) TO ELIMINATE LONGITUDINAL JOINT WHEN INCREASING PANEL SIZE OVER 36SF.
4) AT LOCATIONS WHERE MAINTENANCE EQUIPMENT WILL SUBJECT CONCRETE TO HEAVY LOADS.

ALL SIDEWALK AND BOULEVARD WIDTHS SHALL BE MEASURED FROM BACK OF CURB.
FIELD ADJUST SIDEWALK PROFILES TO MEET ALL DOORWAY THRESHOLDS.

SIDEWALK MUST MAINTAIN POSITIVE DRAINAGE AWAY FROM THE BUILDING TO THE ROADWAY.

SEE SPECIAL PROVISIONS FOR SILICONE SPECIFICATIONS.

- ① LANDING CRITERIA IS REQUIRED FOR ALL DOORS, STEPS, AND PRIVATE WALKS. FEASIBILITY DECREASES WITH NARROWER BOULEVARDS AND STEEPER SIDEWALK PROFILES.
② 18" MIN. WHEN DOOR SWINGS OUTWARD FROM BUILDING. 12" MIN WHEN DOOR SWINGS INWARD FROM BUILDING.
③ 6' MIN. PAR REQUIRED WHEN ADJACENT TO BUILDINGS.
④ 2/3 PAR TO 1/3 BOULEVARD SHOULD BE USED WHEN FEASIBLE. HOLD UNIFORM BOULEVARD WIDTH. 4' PREFERRED MINIMUM BOULEVARD.
⑤ 1%-5% FOR THE MAJORITY OF THE BLOCK, WITH EXCEPTIONS UP TO 8% IN CONSTRAINED AREAS.
⑥ CONSTRUCT USING APPROVED EXPANSION MATERIAL PER MNDOT TYPE A-E EXPANSION. LEAVE A MINIMUM 1/2" TOP GAP AND SEAL WITH MNDOT APPROVED SILICONE PER MNDOT SPEC 3722.
⑦ TO MINIMIZE VIBRATION AND ROLLING RESISTANCE, AREA SHALL BE FREE OF PAVERS, STAMPED CONCRETE, AND/OR EXCESSIVE JOINTING.
⑧ 2% MAX. PER BUILDING CODE. IF GREATER THAN 2%, FLATTEN AS FEASIBLE.
⑨ FORM CONTRACTION JOINTS AS NEEDED TO PRODUCE APPROXIMATELY SQUARE PANEL SIZE. CONCRETE PANEL SIZE SHOULD NOT EXCEED 1 1/2 : 1 LENGTH X WIDTH.
⑩ DRILL AND GROUT NO. 4 X 8" LONG TIE BARS (EPOXY COATED). 36" MAXIMUM SPACING BETWEEN BARS WITH 2" MINIMUM CONCRETE COVER PLACED 1' MINIMUM FROM ADJACENT CONSTRUCTION JOINTS. 1' MINIMUM FROM ADJACENT CONCRETE JOINTS. TIE BARS SHALL BE EMBEDDED 4" WITH 2" MINIMUM CONCRETE COVER AND ARE INCIDENTAL TO SILL PLACEMENT.
⑪ FURNISH AND INSTALL THE FULL WIDTH OF THE TOP OF SILL A MINIMUM 2ML THICK POLYTHENE SHEETING.
⑫ USE AN APPROVED TYPE F (1/4 INCH THICK) SEPARATION MATERIAL. SEPARATION MATERIAL SHALL MATCH FULL HEIGHT DIMENSION OF ADJACENT CONCRETE.
⑬ DIMENSION TO BE SAME AS SIDEWALK THICKNESS, 4" MIN.
⑭ 6" WALK: 5" MIN. FOR B424; 7" MIN. FOR B624
4" WALK: 7" MIN. FOR B424; 9" MIN. FOR B624
⑮ DRILL AND GROUT NO. 4 X 12" LONG TIE BARS (EPOXY COATED). 36" MAXIMUM SPACING BETWEEN BARS WITH 2" MINIMUM CONCRETE COVER PLACED 1' MINIMUM FROM ADJACENT CONCRETE JOINTS.

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Jeffrey J. Perkins
JEFFREY PERKINS
OPERATIONS DIVISION



STANDARD PLAN 5-297.254

4 OF 4

Tom Styrbicki
THOMAS STYRBICKI
STATE DESIGN ENGINEER

APPROVED: 11-04-2021

REVISED:

DRIVEWAY AND SIDEWALK DETAILS

STATE PROJ. NO.

(TH)

SHEET NO.

OF

SHEETS